

DRILL HOLES

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By: Matthew Edelman

GENERAL NOTES:

- THE CONTRACTOR SHALL CHECK ALL DIMENSIONS AND ELEVATIONS PRIOR TO CONSTRUCTION. ANY CONFLICTS CONCERNING THE CONSTRUCTION AROUND EXISTING OBSTRUCTIONS PER THESE PLANS SHALL BE RESOLVED BETWEEN THE CONTRACTOR AND THE FIELD ENGINEER.
- THE CONTRACTOR AND OTHERS SHALL PERFORM ALL WORK IN A MANNER THAT WILL ENSURE THE LEAST PRACTICAL OBSTRUCTION TO TRAFFIC, PEDESTRIANS, RESIDENTS, AND BE CONSISTENT WITH SAFETY.
- BASELINE SHOWN FOR ILLUSTRATIVE PURPOSES ONLY. DIMENSIONS AND STATIONING ARE APPROXIMATE AND NOT A RESULT OF FIELD SURVEY.
- THE CONTRACTOR WILL NOTIFY PROPERTY OWNERS 72 HOURS PRIOR TO RECONSTRUCTION OF DRIVEWAY ENTRANCES.
- THE STATE HIGHWAY ADMINISTRATION OR THE TOWN OF GARRETT PARK DOES NOT WARRANT THE CORRECTNESS OF THE TOPOGRAPHIC OR UTILITY DATA PRESENTED HEREIN AND IS NOT RESPONSIBLE FOR ANY CONCLUSIONS DRAWN FROM THEM.
- SIDEWALK RECONSTRUCTION LIMITS SHALL COINCIDE WITH CRACK CONTROL JOINTS (JOINTER GROOVES) OR EXPANSION JOINTS IN EXISTING SIDEWALK. NEW EXPANSION JOINTS SHALL BE PROVIDED AT THE INTERFACE OF EXISTING AND PROPOSED SIDEWALK. SEE STANDARD MD. NO. 655.01.
- CONTRACTOR TO INSTALL DETECTABLE WARNING SURFACE ON SIDEWALK RAMP AS PER STANDARD MD. NO. 655.40.
- FORM NEW SIDEWALK AND EXPANSION JOINT MATERIAL AROUND EX. UTILITY POLES, MANHOLE COVERS, VALVE COVERS, AND FIRE HYDRANTS AS NECESSARY.
- EXISTING INLETS SHALL NOT BE DAMAGED DURING CONSTRUCTION. DAMAGE TO THE INLETS WILL REQUIRE THE INLET TO BE REPLACED AT NO ADDITIONAL COST TO THE ADMINISTRATION.
- RECONSTRUCT PROPERTY WALKWAYS TO TIE INTO PROPOSED SIDEWALK GRADES AS DIRECTED BY THE ENGINEER.
- EXISTING CURB HEIGHTS PROVIDED ON PLAN SHALL BE VERIFIED BY THE CONTRACTOR. RAMP AND DRIVEWAY TRANSITION LENGTHS SHALL BE VERIFIED IN THE FIELD ACCORDING TO SHA STANDARD MD. NO. 630.01, MD. NO. 630.02, MD. NO. 655.11, MD. NO. 655.12, AND MD. NO. 655.13.
- STABILIZE ALL DISTURBED AREAS NOT DRAINING TO A SEDIMENT CONTROL DEVICE AT THE END OF EACH DAY. THE CONTRACTOR SHALL LIMIT WORK ACTIVITIES SUCH THAT ALL DISTURBED AREAS CAN BE STABILIZED TO FINAL GRADE AND PLACEMENT OF THE TOP SOIL, SEED AND MULCH, SOD OR OTHER STABILIZATION METHODS CAN BE PERFORMED AT THE END OF EACH WORK DAY.
- MAINTAIN ALL VEHICLE ACCESS POINTS AT RESIDENTIAL DRIVEWAYS AND UNSIGNALIZED INTERSECTIONS.
- MAINTAIN IN GOOD CONDITION ALL EXISTING TRAFFIC SIGNS. IF, IN THE OPINION OF THE ENGINEER, ANY SIGNS ARE DAMAGED BY THE CONTRACTOR'S OPERATION, THEY ARE TO BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
- THE CONTRACTOR SHALL ADJUST ALL EXISTING VISIBLE MANHOLES, VALVE BOXES, INLETS AND OTHER STRUCTURES, AS PER SPECIFICATION SECTION 504 AND 875.
- DO NOT LEAVE A PAVEMENT DROP-OFF AT ANYTIME AFTER A WORK DAY IS COMPLETE.
- MAINTAIN THE LIMITS OF WORK WITHIN THE PROJECT IN A SAFE AND NEAT MANNER THROUGHOUT THE DURATION OF THE CONTRACT. PILES OF DEBRIS SUCH AS EXCAVATED SOIL AND HMA ARE EXPECTED TO BE REMOVED OR STABILIZED WITHIN THE SAME WORKING DAY. FAILURE TO COMPLY WITH THIS REQUIREMENT MAY RESULT IN ADMINISTRATIVE ACTION AS OUTLINED UNDER FAILURE TO MAINTAIN PROJECT OF THE SPECIFICATIONS.
- A RIGHT-OF-ENTRY AGREEMENT MUST BE OBTAINED FROM SHA DISTRICT OFFICE PRIOR TO THE COMMENCEMENT OF ANY STAGING ACTIVITIES SUCH AS THE ESTABLISHMENT OF A FIELD OFFICE OR FOR THE STORAGE OF EQUIPMENT AND MATERIAL WITHIN SHA RIGHT-OF-WAY. APPROVAL WILL REQUIRE THE EXECUTION OF THE AGREEMENT AND A RETAINAGE FROM PROGRESS PAYMENTS UNTIL SUCH TIME THE AFFECTED AREA(S) HAVE BEEN RESTORED TO PRE-USE CONDITION. IF ADDITIONAL DISTURBANCE IS NECESSARY, THE CONTRACTOR MAY BE REQUIRED TO OBTAIN ANY APPLICABLE PERMITS THAT MAY BE AFFECTED AND WILL BE SUBJECT TO ANY AND ALL PERMIT CONDITIONS.
- REFER TO ES-01 FOR EROSION AND SEDIMENT CONTROL REQUIREMENTS.
- SEDIMENT AND EROSION CONTROL REGULATIONS WILL BE STRICTLY ENFORCED DURING CONSTRUCTION.
- SIDEWALK RAMP CONSTRUCTION WILL BE PAID AS 5 INCH CONCRETE SIDEWALK.
- WHERE CURB, CURB AND GUTTER, OR DEPRESSED CURB IS PROPOSED, THE CONTRACTOR SHALL REPLACE THE ADJACENT CURB OR CURB AND GUTTER TO THE EXISTING CURB JOINT.
- CONTRACTOR SHALL PROVIDE ENTRANCE TIE-IN AS NECESSARY TO PROVIDE ADEQUATE TRANSITION FROM RECONSTRUCTED ENTRANCE TO EXISTING PAVEMENT (SEE MD 630.01 AND MD 630.02); PAVEMENT MATERIAL SHALL MATCH EXISTING.
- HMA FOR DRIVEWAYS WILL BE PAID AS SUPERPAVE ASPHALT MIX 9.5MM FOR SURFACE, PG 64S-22, LEVEL 4 (2 INCH DEPTH).
- ADJUST EXISTING UTILITIES IMPACTED BY THE SIDEWALK & SIDEWALK RAMP CONSTRUCTION. THE COST IS INCIDENTAL TO SIDEWALK AND SIDEWALK RAMP CONSTRUCTION.
- THE CONTRACTOR SHALL RESET ANY SIGNS DISTURBED BY THE CONSTRUCTION AS DIRECTED BY THE ENGINEER.

MAINTENANCE OF TRAFFIC NOTES:

- MD SHA STANDARD PLATES ARE TO BE USED FOR MAINTENANCE OF TRAFFIC.
- ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE MOT STANDARDS MD 104.00-01 THRU 104.06-25.
- NO WORK IS TO BEGIN UNTIL ALL ADVANCE WARNING SIGNS, DRUMS AND ARROW PANELS ARE IN PLACE AND OPERATIONAL.
- ADVANCED NOTIFICATION OF SIDEWALK CLOSURES SHALL BE PROVIDED. A BARRIER THAT IS DETECTABLE BY A PERSON WITH A VISUAL DISABILITY SHALL BE PLACED ACROSS THE FULL WIDTH OF THE CLOSED SIDEWALK.
- WHERE PEDESTRIANS ARE DIRECTED TO CROSS TO THE OPPOSITE SIDE OF THE ROADWAY, APPROPRIATE ADVANCED WARNING SIGNS SHOULD BE PLACED AT THE INTERSECTIONS RATHER THAN MID-BLOCK CROSSINGS.
- TRAFFIC CONTROL DEVICES AND OTHER CONSTRUCTION MATERIALS AND FEATURES SHALL NOT INTRUDE INTO THE USABLE WIDTH OF THE SIDEWALK, TEMPORARY OR OTHER PEDESTRIAN FACILITY.

65% DESIGN DRAWINGS

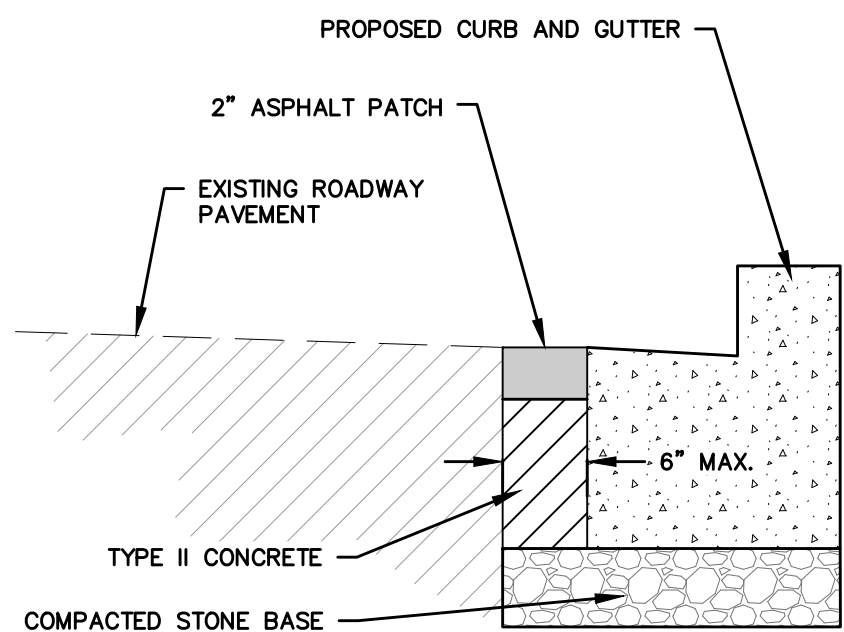


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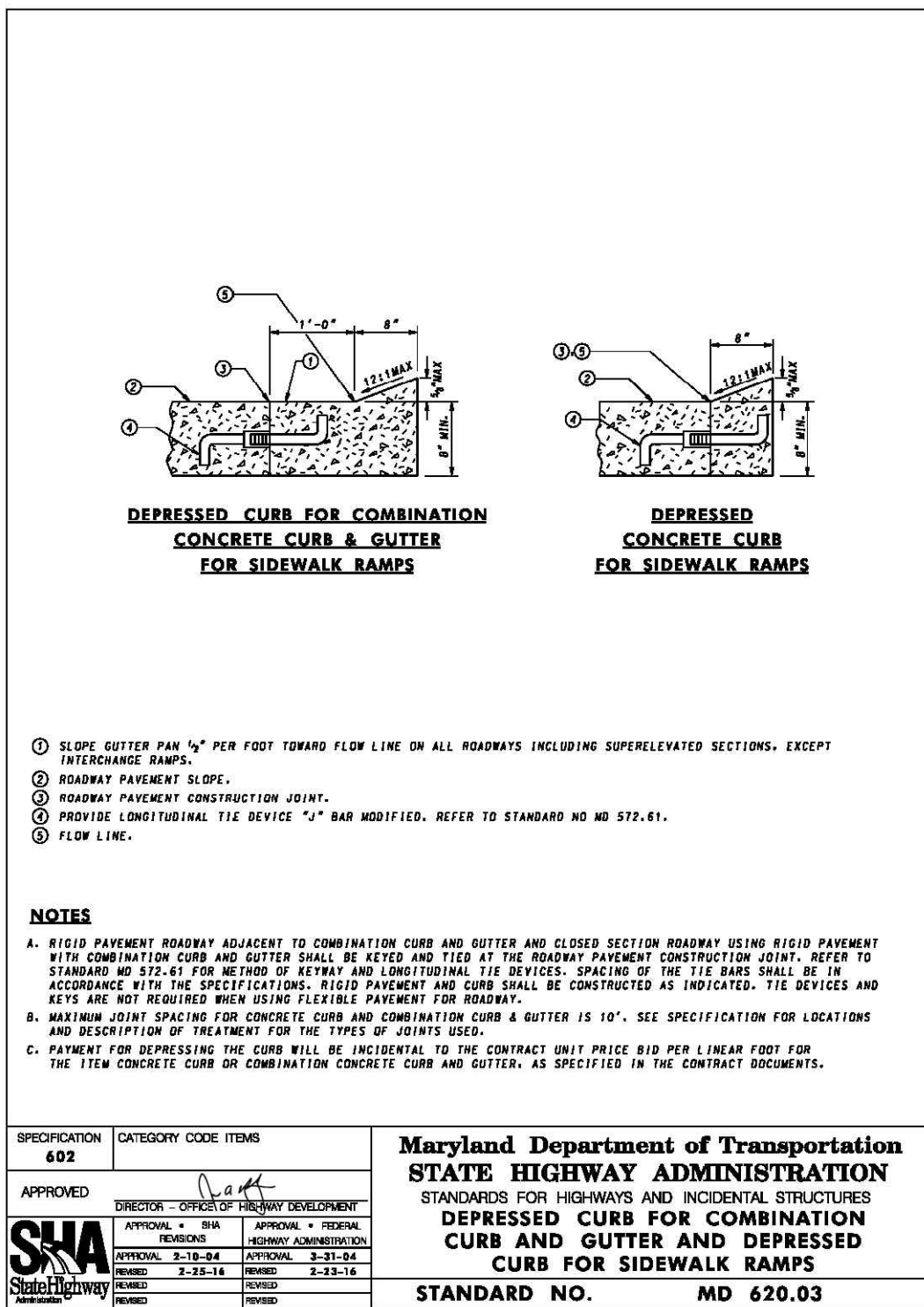
PROFESSIONAL CERTIFICATION:
I CERTIFY THAT THESE DOCUMENTS
WERE PREPARED OR APPROVED BY
ME, AND THAT I AM A DULY LICENSED
PROFESSIONAL ENGINEER UNDER THE
LAWS OF THE STATE OF MARYLAND.
LICENSE NO. 31168
EXPIRATION DATE: 1/12/2019

LEGEND

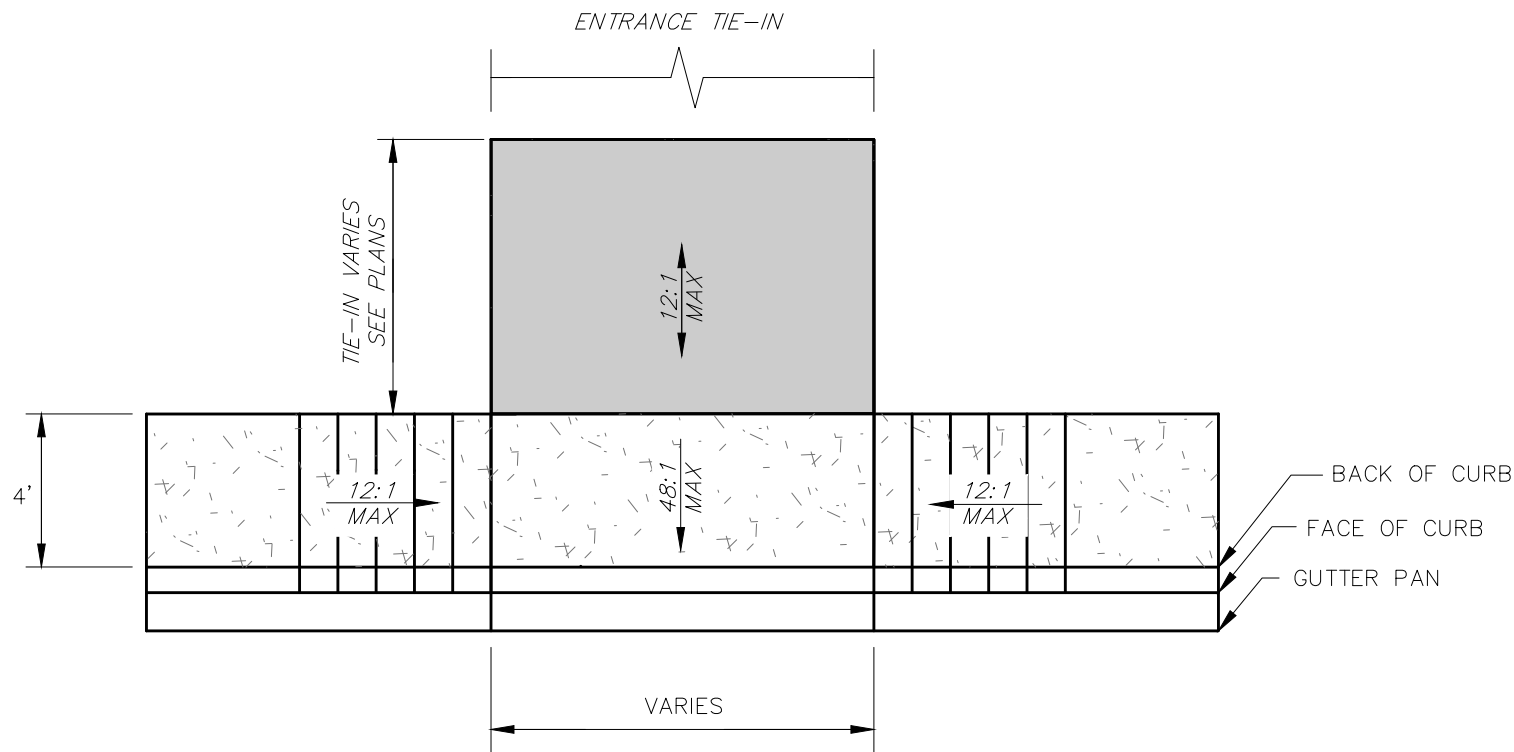
	EXISTING TREE
	EXISTING PINE TREE
	EXISTING SIGN
	EXISTING LIGHT
	EXISTING CURB INLET
	EXISTING SEWER MANHOLE
	EXISTING TRANSFORMER
	FOREST TREELINE
	PROPERTY BOUNDARY
	EXISTING BUILDING
	EXISTING UNDERGROUND ELECTRIC
	EXISTING GAS LINE
	EXISTING SANITARY SEWER
	EXISTING STORM SEWER
	EXISTING CONTOUR
	EXISTING FENCE
	EXISTING CURB
	EXISTING EDGE OF PAVEMENT
	EXISTING SIDEWALK
	EXISTING SPOT ELEVATION
	PROPOSED CONTOUR
	PROPOSED COMBINATION CURB AND GUTTER
	PROPOSED CONCRETE
	PROPOSED ASPHALT



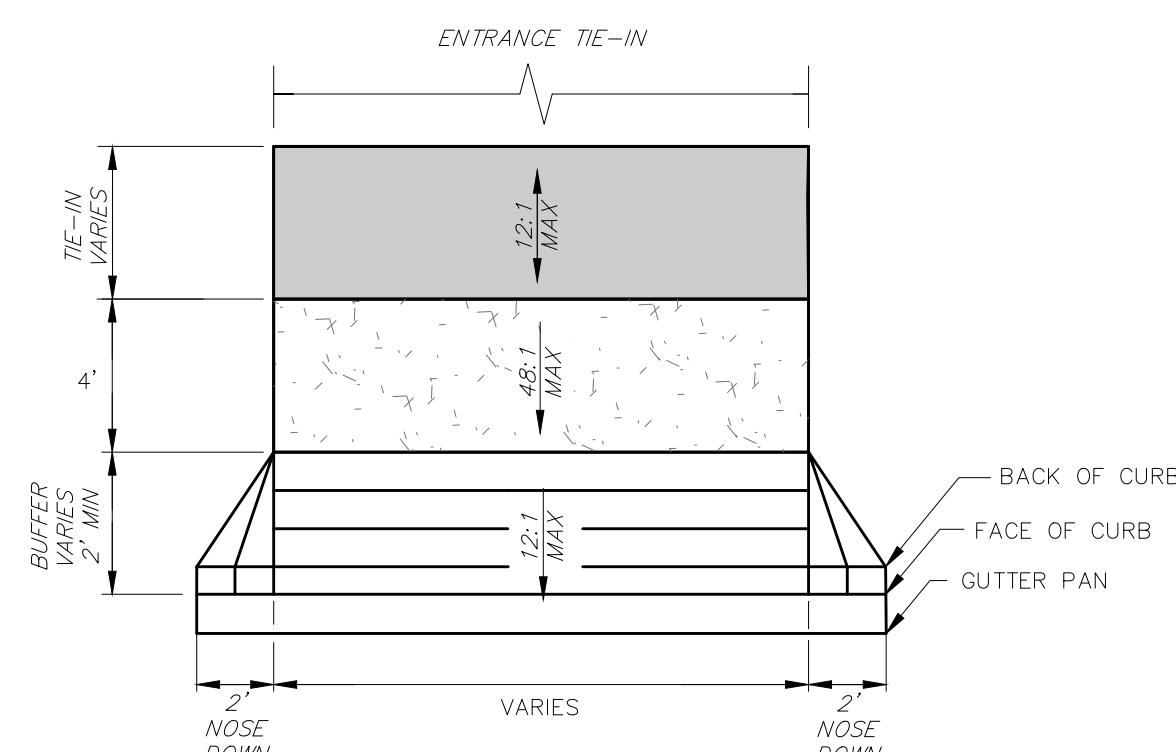
3 TYPICAL PAVEMENT PATCHING DETAIL
NOT TO SCALE



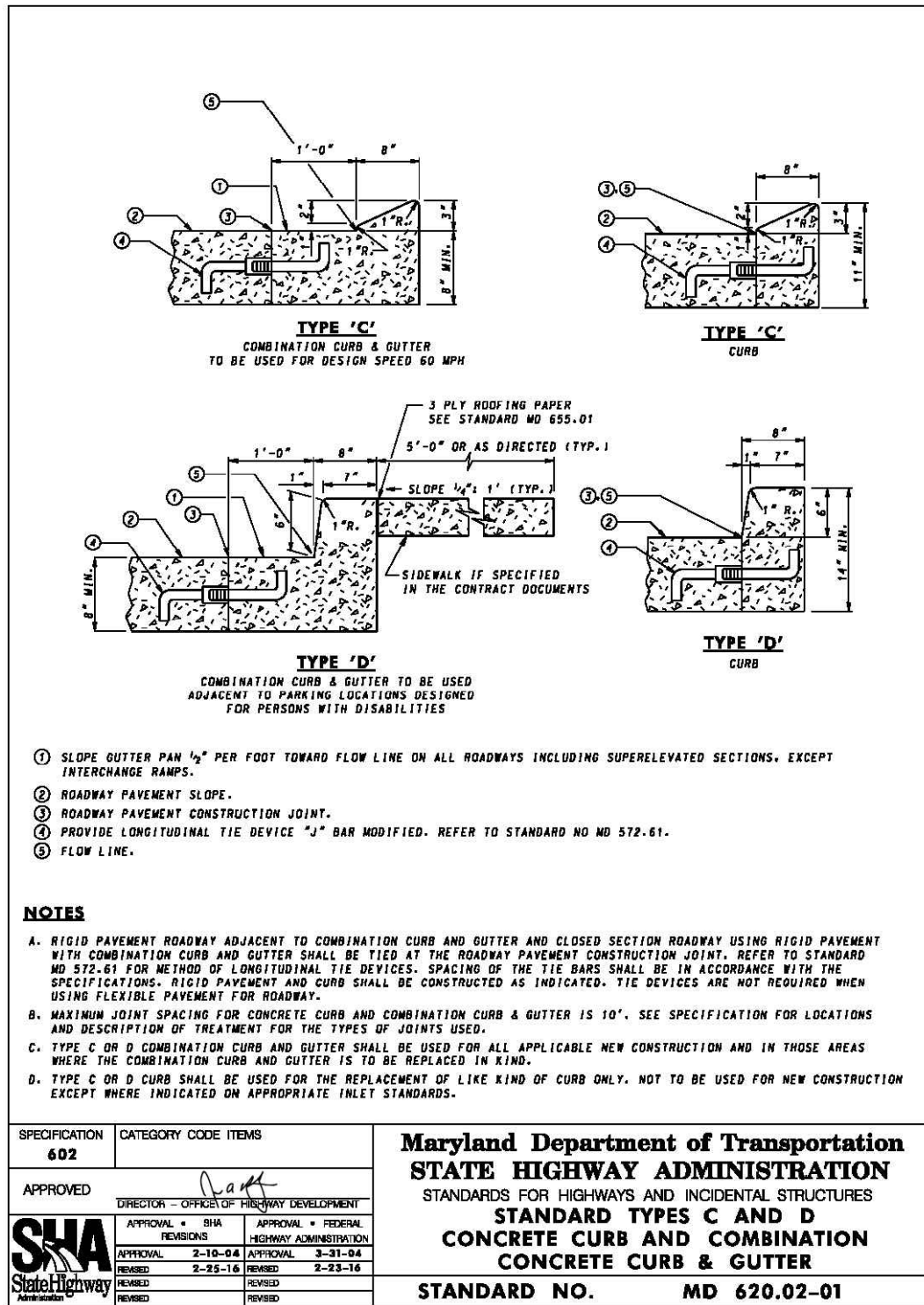
4 TYPICAL DEPRESSED CURB DETAIL
NOT TO SCALE



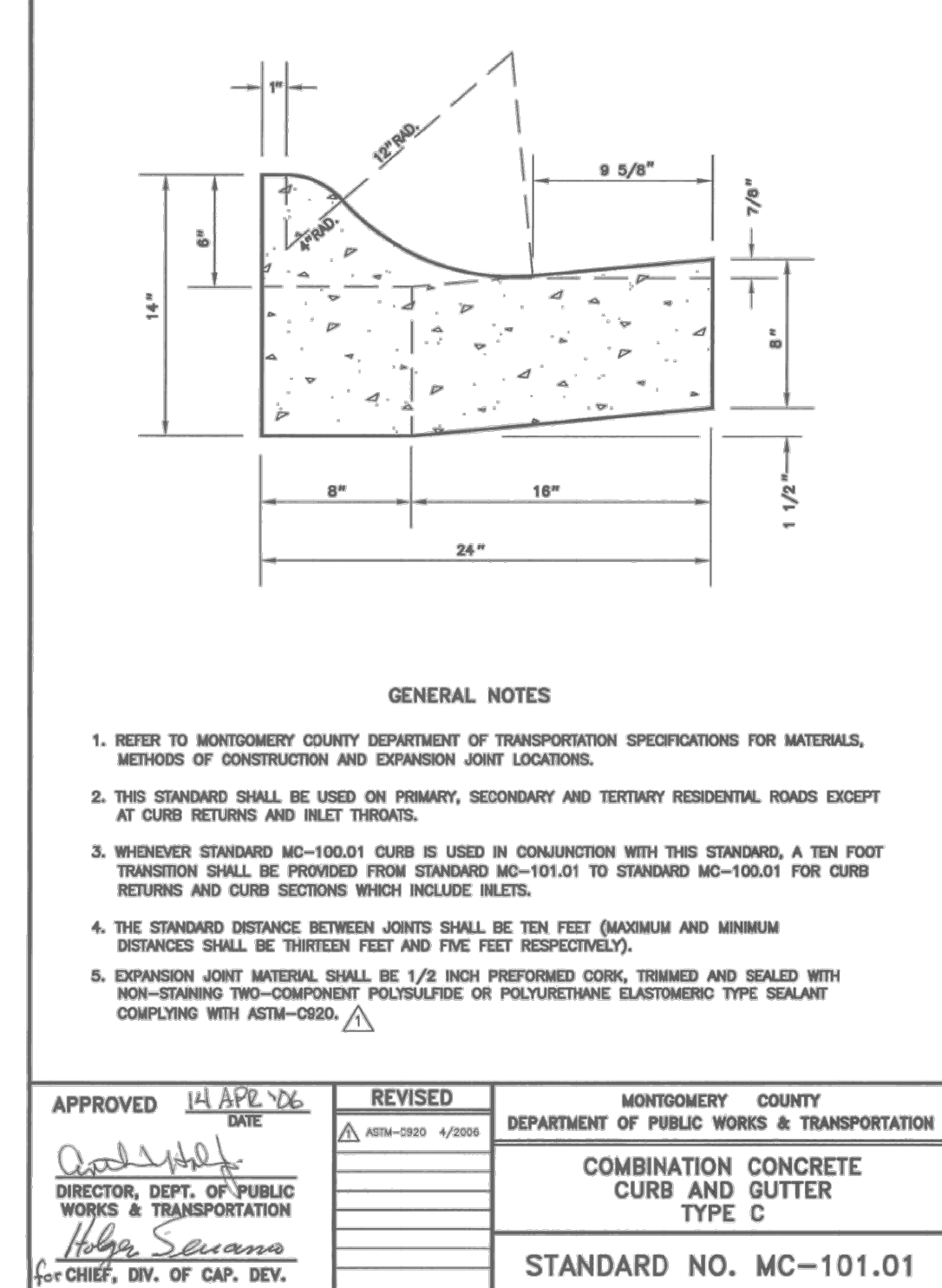
1 TYPICAL DRIVEWAY WITHOUT BUFFER
NOT TO SCALE



2 TYPICAL DRIVEWAY WITH BUFFER
NOT TO SCALE



5 TYPICAL TYPE 'D' COMBINATION CURB AND GUTTER DETAIL
NOT TO SCALE



6 TYPICAL ROLLED CURB DETAIL
NOT TO SCALE

DATUM: NAD 83/91 Horizontal
NAVD 88 Vertical

CROSS REFERENCE	REVISIONS
ITEM	SHEET NOS.
COVER	1
GENERAL NOTES AND DETAILS	2
TYPICAL SECTIONS	3
GEOMETRIC LAYOUT	4 - 5
SIDEWALK PLAN	6 - 10
EROSION & SEDIMENT CONTROL PLAN	11 - 16
EROSION & SEDIMENT CONTROL - DETAILS AND NOTES	17 - 18
STREAM RESTORATION PLAN AND DETAILS	19

PLOTTED: 3/6/2017 11:39 AM
FILE: J:\91140.04 - Safe Routes to School Sidewalks\CAD\dwg\GN01 GENERAL NOTES AND DETAILS.dwg

TOWN OF GARRETT PARK
PEDESTRIAN FACILITY DESIGN SERVICES
SAFE ROUTES TO SCHOOL (SRTS)

GENERAL NOTES AND TYPICAL DETAILS

SCALE	N.T.S.	ADVERTISED DATE	OCT. 2017	CONTRACT NO.	
DESIGNED BY	ME	COUNTY	MONTGOMERY		
DRAWN BY	ME	LOGMILE			
CHECKED BY	JA	HORIZONTAL SCALE			
F.A.P. NO.	SEE TITLE SHEET	VERTICAL SCALE			
DRAWING NO.	GN-01	OF	1	SHEET NO.	X OF X